



Report of the Chief Planning Officer

City Plans Panel

Date: 2nd November 2017

Subject: Planning application reference 17/02666/FU for a ten storey block of 101 apartments with ground floor residents lounge, cinema room, gym and cycle storage room at 16-18 Manor Road, Holbeck, Leeds LS11 9AH

Applicant	Date Valid	Target Date
Citylife Developments	24.05.2017	17.11.2017

Electoral Wards Affected:

City and Hunslet

Yes Ward Members consulted

Specific Implications For:

Equality and Diversity

Community Cohesion

Narrowing the Gap

RECOMMENDATION: DEFER and DELEGATE to the Chief Planning Officer for approval subject to the satisfactory resolution of potential wind impact, and subject to the specified conditions set out in Appendix 1 (and any others which he might consider appropriate) and the completion of a Section 106 agreement to include the following obligations:

- 5% Affordable housing to be provided in accordance with details at section 10.8 of this report
- Sustainable travel fund £14,803
- Car club contribution £10,000
- Amendments to traffic regulation order to replace 3 pay and display bays to accommodate loading and car club £7,500
- Revenue compensation for loss of 3 pay and display bays £18,000
- Travel plan monitoring fee £2505
- Public access through the site
- Cooperation with local jobs and skills initiatives
- Management fee £3000

1.0 Introduction

1.1 This report relates to a full planning application for a ten storey block of 101 apartments with ground floor residents lounge, cinema room, gym and cycle storage room on vacant brownfield land in Holbeck, South Bank in the City Centre.

2.0 Proposal

- 2.1 The proposal is for 101 flats, made up of 21 studio flats, 56 one-bed flats, 20 two-bed flats and 4 three-bed flats. These would be made available to the Private Rented Sector (PRS). No car parking is proposed as part of the scheme. The building would be 10 storeys, including the top 2 storeys set back to create a roof terrace. The architectural concept for the building is a folded sculptural metal form, finished in bronze cladding, with balconies and screens created by the folds in the façade. The former use of the site was a metal fabrication business.
- 2.2 In terms of dwelling size, the following flat sizes would be provided:
- Studio flat 31sqm
 - One-bed flat 39sqm or 41sqm
 - Two-bed flat 63sqm
 - Three-bed flat 74sqm
- 2.3 At ground floor, the entrance to the flats would be on the Manor Road frontage at the western end of the building. The proposal would also include a new pedestrian/cycle link between Manor Road and Siddall Street under an oversailing part of the building. This would create a direct link between David Street and Sweet Street. The route would be activated by a resident's gym, and would be lit and covered by CCTV. The majority of the route would be overlooked by flats at Manor Mills. The proposal would provide additional natural surveillance from the building entrance close to the corner of the building, and from upper floors over the existing indirect pedestrian route that runs to the west of the site.
- 2.4 The application proposal is supported by the following submitted documents:
- Planning Statement
 - Statement of Community Involvement
 - Affordable Housing Statement
 - Affordable Housing Pro Forma
 - CIL / Section 106 Heads of Terms
 - Completed CIL Additional Questions Form and Form 2 (social housing relief)
 - Design & Access Statement incorporating Sustainability Statement
 - Full scaled plans, elevations and sections
 - Planning application form
 - Noise Assessment
 - Transport Statement
 - Wind Study
 - Travel Plan
 - Flood Risk and Drainage Assessment
 - Flood Risk Sequential Assessment Test
 - Phase 1 Ground Report

3.0 Site and Surroundings

- 3.1 The site is currently an unsightly, longstanding cleared vacant brownfield site (0.1 hectare) in the Holbeck Urban Village area of Leeds South Bank, in the designated City Centre. The site is on the south side of Manor Road at the junction with David Street. The immediate surrounding context includes Manor Mills an 8-9 storey residential development to the east, a single storey telecommunications centre (with permission for office use) to the west, and a single storey warehouse to the south. The wider context includes the Dandara residential development, approved at Plans Panel in 2015, and now under construction, and the 8 storey offices at The Mint on Sweet Street. Opposite the site to the east of David Street are 3 storey beige brick offices, and to the west are the single storey industrial units at Leodis Court. The

Grade I listed Temple Works, a former flax spinning mill in the Egyptian revival style, lies at the western end of Manor Road at the junction with Marshall Street, which is also the eastern edge of the Holbeck Conservation Area. The immediate surrounding area offers a wide variety of architectural styles and materials including red brick, light and dark brown brick, render, glazed balconies, copper cladding, and natural stone.

3.2 There is an existing pedestrian route to the west of the site, this is in the ownership of the adjoining landowner. This is not public highway, but it is a claimed Public Right of Way.

3.3 The site lies in flood risk zone 2.

4.0 Relevant Planning History

4.1 Planning reference 20/537/04/FU for an 8 storey block comprising 45 flats, first floor office and ground floor A1/A3 retail/cafe unit and car parking was approved in 2005. This was subsequently amended by planning reference 20/545/05/FU, in order to add 12 additional flats. This second planning permission was approved in 2006, and expired in 2011.

5.0 History of Negotiations

5.1 Pre-application discussions on the current proposals commenced in November 2016. One meeting was held between the agent and architect with planning, highways and design officers. City and Hunslet Ward Councillors were consulted on 8th March 2017. The applicant states that they undertook a pre-application consultation with local residents by posting notices in the reception of Manor Mills and with other local businesses.

5.2 The architect made a pre-application presentation to City Plans Panel on 6th April 2017 and the following issues were discussed:

- The angle of the walls would prevent overlooking of other properties.
- The meshed metal shutter would feature on all sides of the building.
- The height of the link through the building was felt to be sufficient and would allow the flow of natural light.
- Affordable housing – whilst the applicant would like to commit to on-site provision of affordable housing, experience on similar projects had shown this to be difficult due to the low number of units available and the difficulty in engaging a social landlord to manage these. It was considered that a commuted sum may be the most appropriate route. It was further mentioned that there could be other options through buy to rent or sub market discounted housing. Members expressed concern regarding the lack of on-site affordable housing.
- Footpaths around the site and relation to adjacent sites which would be subject of future development.
- The cladding would be a copper coloured aluminium and with correct maintenance would retain its quality of finish. There would not be an issue with reflections from car headlights as the lower part of the building would not have the cladding finish.

Members commented as follows:

- Members supported the size and quality of residential dwellings proposed.
- Members agreed that the proposed building layout, height and emerging design was generally acceptable.
- With regard to the lack of on-site parking there was some concern that this could cause problems to neighbouring areas. Questions were asked whether

basement parking could be provided. It was stated that basement parking would not be feasible due to space. It was considered that due to the location of the development that a no car scheme was potentially suitable. Further consideration to this matter would be given as part of the Transport Assessment at the next stage of the application.

6.0 Public/Local response

6.1 Planning Application publicity

- Site Notice 09.06.2017
- Press Notice 09.06.2017
- City and Hunslet Ward Councillors consulted 31.05.2017

6.2 1 comment from Leeds Civic Trust (LCT)

- LCT support the proposed use and the design of the building but object to its height. The design and cladding of the building will give it a distinctive appearance, differentiating it from the rather indifferent designs of recent similar buildings around it. Its smaller footprint than most other developments of this type also provides some variety. However, the Holbeck SPD, proposes that buildings should be reduced in height the closer they are to the Grade I listed Temple Mill, guidance which the Trust supports. Logically the proposed building should therefore be lower than the adjacent Manor Mills building, but is in fact two storeys, 5m, higher. The fact that the cladding height is the same as Manor Mills and the additional storeys are set back slightly does not mitigate this anomaly, where it is building outline that is important, not façade alignments.
- LCT are particularly supportive of the proposal to open up a public pedestrian route through the building.

6.3 3 objections have been received from residents at Manor Mills, expressing the following concerns:

- Impact on daylight/sunlight and visual dominance. It is not clear how the proposal will impact on the views of the existing dwellings at the northern internal corner of Manor Mills and the Design and Access Statement fails to provide a visual of this relationship. The proposed development is greater in height and is as such overbearing in nature. There will be a very significant loss of natural daylight and views for apartments facing the proposed development.
- The proximity of the proposed building is too close to Manor Mills, creating a major privacy concern. A distance of 28 metres cannot possibly be considered a large separation.
- Noise pollution from the proposed building will be intrusive, disrupting sleep for residents during the build and into the future.
- The gaudy bronze cladding is not aesthetically pleasing, rather appearing rusted. Further, the glare from the metal surface is liable to cause problems for motorists, pedestrians and Manor Mill residents alike.
- The building is at odds with the architecture of the area, it seems no attempt has been made to make it fit in with its surroundings.
- The visuals show the proposed building linked to the side of Manor Mills: is that intentional, or a design error
- An increase in traffic to the area is a concern.
- There is likely to be an increase in litter issues - Leeds Council already struggles to keep the area tidy.
- The land is better suited for parking spaces, a garden, or a single storey building.
- Concern about lack of pre-application consultation with leaseholders
- Impact on the setting of the Grade I listed Temple Works due to height and lack of a heritage assessment in the planning application

- Private rented sector housing will create a transient population which could damage the long term sustainability of Holbeck Urban Village.

7.0 Consultations

7.1 Statutory Consultations

7.1.1 Historic England

At the time of writing this report, Historic England comments are awaited. Any comments received will be updated verbally at Panel.

7.1.2 Coal Authority

The application site does not fall within the defined Development High Risk Area. There is no requirement in this case to consult the Coal Authority.

7.1.3 Environment Agency

No objection subject to passing the flood risk sequential test, and conditions regarding the implementation of the measures set out in the submitted flood risk assessment

7.2 Non-statutory Consultations

7.2.1 Leeds City Council (LCC) Transport Development Services

Highways officers have advised that the proposal is acceptable in principle, subject to the provision of identified off-site highways works including informal pedestrian crossing provision and traffic calming measures at the junction of David Street with Manor Road, improvements to the north end of Siddall Street, provision of a loading bay on Manor Road and provision of a car club bay, reinstatement of footway and full height kerbs at redundant vehicle accesses and associated new/amended TRO's the inclusion of the Travel Plan in the Section 106 agreement, a sustainable travel fund, and secure cycle parking.

7.2.2 LCC Flood Risk Management

No objection subject to a condition that requires the surface water greenfield discharge rate to be applied (5l/s per hectare) unless it can be justified it is not feasible.

7.2.3 LCC Public Rights of Way

A claimed footpath runs from Manor Road to Siddall Street [along the western and southern edge of the site outside the application boundary]. This footpath is well used and looks to have been laid out for the public to use with a tarmacked and flagged surface. The surface is rough and in a general state of disrepair. As the development is likely to cause an increase in use by the public the footpath should be resurfaced for the betterment of the development and the wider community. The footpath should have formal status by adoption as a highway or recorded as public right of way by Creation Agreement, this will avoid problems over appropriate use and responsibility for maintenance in the future.

7.2.4 West Yorkshire Combined Authority (WYCA)

To encourage the use of sustainable transport as a realistic alternative to the car, the developer needs to fund a package of sustainable travel measures. WYCA recommend that the developer contributes towards sustainable travel incentives to encourage the use of sustainable modes of transport. Leeds Council have recently introduced a sustainable travel fund. The fund can be used to purchase a range of sustainable travel measures including discounted MetroCards (Residential MetroCard Scheme) for all or part of the site. This model could be used at this site. The payment schedule, mechanism and administration of the fund would have to be

agreed with Leeds Council and WYCA and detailed in a planning condition or S106 agreement. As an indication of the cost should the normal RMC scheme be applied based on a bus only ticket, the contribution appropriate for this development would be £49,606.15. This equates to Bus Only Residential MCards.

- 7.2.5 West Yorkshire Police
Advice given regarding security, access control, cycle parking and CCTV in the interests of the safety of future residents.
- 7.2.6 LCC Nature Conservation
There should be no significant nature conservation concerns provided the site is not cleared between 1st March and 31st August unless a competent ecologist has checked for active birds nests within 24 hours of workings commencing, in order to protect nesting birds in accordance with the Wildlife and Countryside Act 1981 (as amended).
- 7.2.7 LCC Environmental Protection
No objection subject to the implementation of the sound insulation measures detailed in the submitted noise report.
- 7.2.8 LCC Conservation Team
No objection.

8.0 Planning Policy

8.1 Statutory Context

- 8.1.1 Section 38(6) of the Planning and Compulsory Purchase Act 2004 requires the application to be determined in accordance with the development plan unless material considerations indicate otherwise. For the purposes of decision making, the Development Plan for Leeds currently comprises the following documents:
- The Leeds Core Strategy (Adopted November 2014)
 - Saved UDP Policies (2006), included as Appendix 1 of the Core Strategy
 - The Natural Resources & Waste Local Plan (NRWLP, Adopted January 2013) including revised policies Minerals 13 and 14 (Adopted September 2015).

Development Plan

8.2.1 Leeds Core Strategy 2014

The Core Strategy sets out strategic level policies and vision to guide the delivery of development investment decisions and the overall future of the district. Relevant Core Strategy Policies include:

- Spatial policy 1 location of development
- Spatial policy 3 role of Leeds City Centre
- Spatial policy 6 housing requirement and allocation of housing land
- Spatial policy 8 economic development priorities
- Policy CC1 City Centre development
- Policy CC2 City Centre south
- Policy CC3 improving connectivity between the City Centre and neighbouring communities
- Policy H3 density of residential development
- Policy H4 housing mix
- Policy H5 affordable housing
- Policy EN1 carbon dioxide reduction
- Policy EN2 sustainable design and construction

- Policy EN4 district heating
- Policy EN5 managing flood risk
- Policy G9 biodiversity improvements
- Policy P10 design
- Policy P11 heritage
- Policy P12 landscape
- Policy T1 transport management
- Policy T2 accessibility requirements and new development

8.2.2 **Leeds Unitary Development Plan Review 2006 Saved Policies**

Relevant saved policies include:

- Policy GP5 all relevant planning considerations
- Policy BD2 new buildings
- Policy BD5 residential amenity
- Policy LD1 landscaping
- CC31A Holbeck Urban Village Strategic Housing and Mixed Use Site

8.2.3 **Leeds Natural Resources & Waste Plan 2013**

The Natural Resources and Waste Development Plan Document (Local Plan) is part of the Local Development Framework. The plan sets out where land is needed to enable the City to manage resources, like minerals, energy, waste and water over the next 15 years, and identifies specific actions which will help use natural resources in a more efficient way. Relevant policies include:

- Minerals 2 minerals safeguarding area sand and gravel
- Minerals 3 mineral safeguarding area coal
- Air 1 management of air quality through new development
- Water 1 water efficiency
- Water 2 protection of water quality
- Water 4 development in flood risk areas
- Water 6 flood risk assessments
- Water 7 surface water run-off
- Land 1 contaminated land

8.3 **Relevant Supplementary Planning Guidance includes:**

Leeds Parking SPD

Street Design Guide SPD

Neighbourhoods for Living SPG

Travel Plans SPD

Accessible Leeds SPD

Building for Tomorrow Today: Sustainable Design and Construction SPD

Holbeck South Bank SPD 2016

8.4 **Holbeck South Bank SPD 2016**

The Holbeck South Bank SPD was adopted in June 2016 as an updated revision to the 1999 and 2006 Holbeck Urban Village Planning Frameworks. As with earlier versions the main aim of the SPD is to create vibrant, sustainable, mixed use communities whilst safeguarding the unique historic character of the area. The whole of Holbeck, South Bank, is designated as a mixed-use area which should include a mixture of working, living, retailing and recreational opportunities. It is intended that the area should meet some of the identified need for City Centre housing. Further improvements to connectivity are encouraged. A pedestrian and cycling friendly environment is sought in part by minimising through traffic in the area. Encouragement will also be given to developing with the minimum acceptable parking provision. The site falls within the Eastern Gateway character area. It may be appropriate for new buildings to gradually increase in height away from the

listed buildings. Buildings should define street frontages and provide pavement widths responding to building scale. New footpath links are encouraged and new public realm should be given an adequate sense of enclosure by the buildings that define them. Relevant specific proposals for this area include the extension and enhancement of the footpath linking Manor Road to Siddall Street as part of a new network of routes between Manor Road, Sweet Street and Marshall Street.

8.5 **National Planning Policy Framework (NPPF)**

The NPPF identifies 12 core planning principles (para 17) which include that planning should:

- Proactively drive and support sustainable economic development to deliver homes;
- Seek high quality design and a good standard of amenity for existing and future occupants; and
- Actively manage patterns of growth to make the fullest possible use of public transport, walking and cycling.

Paragraph 49 states that housing applications should be considered in the context of the presumption in favour of sustainable development. The NPPF states that local authorities should deliver a wide choice of homes, widen opportunities for home ownership and create sustainable, inclusive and mixed communities (para 50).

Section 7 states that good design is a key aspect of sustainable development, is indivisible from good planning, and should contribute positively to making places better for people. It is important that design is inclusive and of high quality. Key principles include:

- Establishing a strong sense of place, using streetscapes and buildings to create attractive and comfortable places to live, work and visit;
- Optimising the potential of the site to accommodate development;
- Respond to local character and history;
- Reflect the identity of local surroundings and materials, while not preventing or discouraging appropriate innovation;
- Create safe and accessible environments; and
- Development to be visually attractive as a result of good architecture and appropriate landscaping.

Paragraph 61 states that although visual appearance and the architecture of individual buildings are very important factors, securing high quality and inclusive design goes beyond aesthetic considerations. Therefore, planning policies and decisions should address the connections between people and places and the integration of new development into the natural, built and historic environment.

Paragraph 131 states that in determining planning applications local planning authorities should take account of the desirability of sustaining and enhancing the significance of heritage assets, the positive contribution that conservation of heritage assets can make to sustainable communities including their economic viability, and the desirability of new development making a positive contribution to local character and distinctiveness.

Paragraph 132 of the NPPF states that when considering the impact of a proposed development on the significance of a designated heritage asset great weight should be given to the assets conservation, and that the more significant the asset the greater the weight should be. It also states that significance can be harmed through development within its setting, and that substantial harm to or loss of designated

heritage assets of the highest significance (Grade I and II* listed buildings and Scheduled Ancient Monuments) should be wholly exceptional.

Paragraph 134 states that where a development proposal will lead to less than substantial harm to the significance of a designated heritage asset, this harm should be weighed against the public benefits of the proposal, including securing its optimum viable use.

Paragraph 137 states that Local planning authorities should look for opportunities for new development within Conservation Areas and World Heritage Sites and within the setting of heritage assets to enhance or better reveal their significance. Proposals that preserve those elements of the setting that make a positive contribution to or better reveal the significance of the asset should be treated favourably.

8.6 National Planning Practice Guidance (NPPG)

This provides Central Government Guidance on a range of planning matters and provides the following advice:

“The private rented sector

Some privately rented homes can come from purpose built schemes held in single ownership which are intended for long term rental. The economics of such schemes differ from build to sale and should be determined on a case by case basis. To help ensure these schemes remain viable while improving the diversity of housing to meet local needs, local planning authorities should consider the appropriate level of planning obligations, including for affordable housing, and when these payments are required. So these homes remain available to rent only, Local Planning Authorities may choose to explore using planning obligations to secure these schemes for a minimum period of time. Local Planning Authorities should enforce these planning obligations in the usual way.”

8.7 Other material considerations

8.7.1 Emerging Site Allocations Plan (SAP)

The site is allocated for housing in the emerging SAP for 57 flats under site reference HG1-463.

8.7.2 Draft Holbeck Neighbourhood Plan

Although the draft Plan area is to the south of Sweet Street and does not directly affect this site, a key objective of the plan is to seek better connections for pedestrians, cyclists and public transport in the area between Holbeck and the City Centre. The draft Neighbourhood Plan was subject to public consultation in 2016, however it has little planning weight at this stage.

8.7.3 The Leeds Standard and the DCLG Technical Housing Standards

The Leeds Standard sets out the importance of excellent quality housing in supporting the economic growth ambitions of the Council. The Leeds Standard sizes closely reflects the Government’s Technical Housing Standards – Nationally Described Space Standard which seek to promote a good standard of internal amenity for all housing types and tenures. Whilst neither of these documents has been adopted as formal planning policy in Leeds given their evidence base in determining the minimum space requirements they are currently used to inform decisions on the acceptability of development proposals. The Council has committed to prepare a Development Plan Document (DPD) which will allow the national standards to be applied to new housing development in Leeds. This is

programmed to be incorporated within the Core Strategy selective review, with public consultation taking place later this year.

- 8.7.4 Private Rented Sector (PRS) / Build To Rent (BTR) and Affordable Housing
On 22 March 2017 Leeds City Council's Executive Board endorsed an approach which recognises that the acceptance of commuted sums from PRS/BTR schemes may be appropriate and justified in accordance with Core Strategy Policy H5.

9.0 Main Issues

- 9.1 Principle of the proposed use
- 9.2 Residential quality and sustainability
- 9.3 Design, scale and layout
- 9.4 Highways and transportation
- 9.5 Flood risk and sustainable drainage
- 9.6 Wind
- 9.7 Other considerations
- 9.8 Planning obligations and Community Infrastructure Levy (CIL)

10.0 Appraisal

10.1 Principle of the proposed use

- 10.1.1 The redevelopment of the brownfield site in a highly sustainable City Centre location would meet the Core Strategy objectives including policies SP3, CC1, H2, CC2, and CC3, by regenerating a vacant brownfield site, delivering new homes, and new pedestrian connections. It would also meet the aims of the existing saved site allocation (CC31A) and the emerging Site Allocations Plan (HG1-463). The proposal would contribute to meeting the adopted Holbeck South Bank SPD vision for the area, to create a mixed-use City Centre neighbourhood, including a mixture of working, living, retailing and recreational opportunities.

10.2 Residential Quality and Sustainability

- 10.2.1 The majority of the accommodation would meet the Nationally Described Housing Standards, with the exception of the studio flats which would be 31sqm. These are similar in size to those approved and under construction at the nearby Dandara site to the east along Manor Road. However, it is considered that the proposed layout, daylight, circulation and juxtaposition of living functions in these flats would be satisfactory and on balance it is considered that these are acceptable.
- 10.2.2 Core Strategy Policy H4 on Housing Mix requires a minimum provision of 20% of flats be provided containing 3 bedrooms across the Local Authority area. However, the policy allows the nature and location of the scheme to be taken in to account when assessing housing mix. Given that this scheme offers approximately 4% three-bed flats, and the provision of three-bed dwellings in the city centre currently stands at around 1%, this proposal would exceed the percentage of current provision in the area. Generally a 5% provision of three bed flats has been achieved in most major city centre residential planning applications approved since the adoption of the Core Strategy. On balance it is considered that the proposed mix would offer a sufficient variety in unit type for a scheme of this size in this City Centre location.
- 10.2.3 The application is supported by a Sustainability Statement, which sets out how 10% on-site low carbon energy generation would be achieved on site with roof-top solar panels, and confirms that 20% betterment on carbon dioxide emissions above the 2013 Part L building regulations, and the water usage target, would be achieved on-

site. These measures are required by Core Strategy Policies EN1 and EN2, and would be controlled by planning condition.

10.3 **Design, scale and layout**

- 10.3.1 The Holbeck South Bank SPD 2016 (Part 2, Section 11 Eastern Gateway area p42-3) states that there is potential for a building at this site to be 7-9 storeys in height. Whilst this is a guideline, there is a generally consistent emerging height and building line to this part of Manor Road, formed by City Walk and Velocity in the east, the Dandara scheme that is under construction (14/04641/FU), and the Manor Mills flats adjacent to this site. The remainder of Manor Road to the north and west is low rise. It is considered that development along Manor Road should generally step down towards the Grade I listed Temple Mill, which is framed at the western end of the street. Marshall Street is also the boundary of the Holbeck Conservation Area, which lies to the north and east of the site. The increase in height by one/two storeys to ten storeys (approximately 30m) adjacent to Manor Mills, which is part eight/part nine storeys, would act as a focal point at the junction of David Street, and due to the setting back of the top two storeys, is not considered to be a sufficient increase in scale to be out of character with the emerging streetscene along Manor Road from the Dandara flats, Manor Mills and the application proposal. There is also sufficient distance, approximately 100m, to the listed Temple Mill to avoid an overbearing and overdominant effect on its setting. Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that Local Planning Authorities (LPAs) must have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses and pay special attention to the desirability of preserving or enhancing the character and appearance of a Conservation Area. There is a variety of heights and scale in the area, and the general built form along this part of the block between Manor Road and Sweet Street is around 30 metres in height. Buildings generally step down in scale to Temple Mill and the western end of Manor Road. It is not considered that the proposal, would obscure or interrupt views of the principal interest facades of Temple Mill from Manor Road, would not adversely affect the special character or significance of the setting of the Grade I listed building. It is considered that the regeneration of the site with a high quality well-designed building would enhance the setting and significance of Temple Mill, and the character and appearance of the nearby Holbeck Conservation Area, in accordance with Core Strategy Policies P10 and P11.
- 10.3.2 It is considered that the proposed angled bronze cladding treatment is positive and imaginative, and would in principle be supported. The architectural treatment would provide visual interest to Manor Road, by contrasting with the recent developments, and enhancing the character of the area, and the setting of Temple Mill in longer views along the street. In response to objector comments about the risk of glare from the sun, the architect has confirmed that the bronze cladding panel would have a matt finish, which would minimise this risk. Exact details and large-scale samples of cladding materials would be controlled by planning condition.
- 10.3.3 The Holbeck South Bank SPD promotes a series of new pedestrian routes and spaces, alongside surrounding buildings and infrastructure. The provision of a new more direct pedestrian route through the building, at a width of approximately 6m, would be additional to the existing pedestrian route adjoining the site. The new route would promote better links between Water Lane, David Street and Sweet Street, as a contribution to a wider set of new connections encouraged by the SPD.
- 10.3.4 Three local residents have raised concern regarding the impact of the proposal on potential for overlooking and loss of privacy, visual dominance, outlook,

overshadowing and loss of light to flats at Manor Mills. The building would be constructed next to the blank western gable of Manor Mills, and would then step in by some 6m away from the shared boundary. The distance between the west-facing elevation with windows of Manor Mills and the eastern flank of the proposal would be approximately 28m, which is considered to be acceptable in a City Centre environment, between buildings of a similar scale, in terms of privacy, outlook, daylight and sunlight. Whilst the building would have a visual impact on the outlook of south facing flats on the northern wing of Manor Mills, it is considered that there would be sufficient separation across the new public route between the two buildings at this point. Given the orientation of the south facing flats at Manor Mills, whilst there may be some loss of sunlight it is considered that there would be sufficient daylight and that there would not be a significant adverse impact on residential amenity in this case. It is considered that there would be satisfactory privacy relationships between the proposed building and its neighbours in terms of distances between facing habitable room windows across and around the scheme. Within the context of a dense edge of centre urban grain it is considered that the distances between habitable room windows are generally acceptable and allow sufficient clear outlook within the site boundary. The south facing north wing of Manor Mills would be over 6m away from the proposal as it runs southwards, however the shutter design on the windows of the application proposal would limit overlooking between the two buildings. It is considered on balance in this context that the proposal would meet Core Strategy Policies P10 and CC1 and Saved UDPR Policies GP5 and BD5.

10.4 **Highways and transportation**

10.4.1 The Holbeck South Bank SPD highlights the desire to produce a pedestrian and cycling friendly environment, in part by minimising through traffic in the area and by developing with the minimum acceptable parking provision. The site is located in a sustainable City Centre location within a 10 minute walking distance of employment, retail, leisure, supporting facilities and public transport including Leeds Station Southern Entrance and bus stops along Meadow Lane. The scheme would therefore meet the Policy T2 and the Accessibility Standards in Table 2 of Appendix 3 of the Core Strategy. The site is also within a controlled and enforced parking area, with pay and display spaces on the street in the vicinity of the site. The scheme proposes no car parking spaces. The scheme would be supported by an acceptable travel plan which would be attached to the Section 106 legal agreement. Also included in the legal agreement is the provision of a Sustainable Travel Fund for the site, which would include car club trial provision for residents. In relation to WYCA comments, Travel Plan measures have been agreed with the applicant, and a Sustainable Travel Fund sum has been calculated based on the latest Travel Plan guidance. The Sustainable Travel Fund sum is based on half the equivalent cost of residential metrocards in a City Centre location, to take account of the increased likelihood that City Centre residents would walk or cycle to work. Part of the sum is also identified for City Car Club trial provision, in this case £10,000. The Travel Plan Coordinator has the option of purchasing metro cards with the remaining £14,803 Sustainable Travel Fund sum if these are appropriate to the residents at the time. Secure long stay cycle parking would also be provided on-site. On this basis, it is considered that a zero car parking provision would be acceptable in this sustainable City Centre location, where on-street parking is managed and enforced.

10.4.2 Highways officers have confirmed that the scheme is acceptable in terms of arrangements for deliveries and refuse/recycling servicing. In order to facilitate the off-site highways works to Manor Road and David Street which would provide a loading bay and deliver improvements for pedestrians, there would also be a contribution required for changes to Traffic Regulation Orders and compensation for the loss of on-street parking bays. Dropped kerbs and tactile paving would be

required for informal road crossing points, to connect pedestrian route desire lines in the area. Reinstatement of footways with full height kerbs for any redundant accesses would also be required. A planning condition would control the off-site highways works, and it is therefore considered that the proposal would meet Core Strategy Policies T1 and T2.

10.5 Flood risk and sustainable drainage

10.5.1 The application has been supported by information which adequately demonstrates that the NPPF flood risk sequential test has been passed. The site is proposed for residential development however it is in flood zone 2 and therefore an exception test is not required.

10.5.2 The following flood mitigation measures are proposed in the submitted Flood Risk Assessment (FRA):

- All apartments would be provided at first floor level and above which will be significantly higher than the design flood level (1in100yr + climate change), the 1in1000yr flood level and the existing ground level.
- The ground floor would be occupied by a gym, cinema, cycle store, bin store and other ancillary uses and would be set at a minimum level of 28.4mAOD which is approximately 300mm higher than the existing ground levels (28.1mAOD) and allows for 400mm of freeboard above the 1in100yr + climate change flood level (27.996mAOD). This level is also less than 500mm below the 1in1000 yr flood level (28.866mAOD).
- The nature of the development would mean that residents could escape to the upper floors of the building if necessary in the event of a flood where they could remain until the flood waters recede. The applicant would also prepare a flood warning and evacuation plan, and sign up to Environment Agency flood alerts.
- External ground levels would be designed to direct any surface water flow away from building thresholds.

10.5.3 The applicant has submitted a sustainable drainage strategy (SuDS), which sets out the following measures:

- Post-development surface water run-off rate should be restricted to the greenfield run-off rate if practicable.
- Unless an area is designed to hold and/or convey water, flooding does not occur on any part of the site for a 1 in 30-year rainfall event.
- Flooding does not occur during a 1 in 100-year storm event (including an allowance for the anticipated impacts of climate change) in any part of a building (including a basement) or in any utility plant susceptible to water (e.g. pumping station or electricity substation) within the development.
- Flows resulting from rainfall in any event exceeding the 1 in 100-year rainfall event are managed in exceedance routes to minimise as far as practicable the risk of flooding to people and property both on and off site.
- The SuDS design for the development site should ensure that the quality of any receiving water body is not adversely affected and preferably enhanced.

10.5.4 The Environment Agency and Leeds City Council Flood Risk Management have no objection to the application proposal subject to conditions regarding the implementation of the measures set out in the flood risk assessment and details of the surface water drainage. It is therefore considered that the submitted flood risk and drainage proposal would comply with the requirements of the NPPF, Core Strategy policy EN5 and NRWDPD policies Water 4, 6, and 7.

10.6 Wind

10.6.1 The applicant has submitted a quantitative wind report in support of the application, which has been independently peer reviewed by a qualified and experienced wind consultant on behalf of the Council. Based on the modelling provided, a potential safety concern has been highlighted at the entrance to the flats, because the safety threshold for entrances could be exceeded for vulnerable users in the windiest predicted conditions. In this case, it is likely that mitigation measures would be required to reduce this risk, such as the reconfiguration of the entrance ramp and steps. Discussions with the applicant are on-going at the time of writing. It is requested that Members defer and delegate approval to the Chief Planning Officer, in order to progress the satisfactory resolution of this matter.

10.7 Other considerations

10.7.1 In relation to one of the objector's concern about lack of pre-application consultation with leaseholders, the planning application was publicised on 9th June 2017 with 3 site notices (Siddall Street, Manor Road and on the pedestrian link close to the residential entrance to Manor Mills), and a press notice in accordance with statutory requirements. Residents and leaseholders can also sign up to alerts on Public Access on the Council's website to be notified when planning applications are submitted in their area. In addition, the applicant's agent states that residents were notified by the agent at pre-application stage via leaflets in the entrance to the flats. It is very difficult to establish all possible land interests within a block of flats and consult with all potential freeholders, leaseholders and tenants, and therefore this is not standard practice for planning application publicity.

10.7.2 Whilst concerns regarding visual dominance, loss of daylight and outlook, and construction noise and disturbance are acknowledged, the present open aspect and lack of activity to the west enjoyed by Manor Mills is a temporary condition, pending redevelopment in accordance with longstanding adopted policy objectives and the future regeneration of the Holbeck/South Bank area. A condition is recommended to control construction activities, working and delivery hours, in order to minimise the potential for harm to amenities.

10.8 Planning obligations and Community Infrastructure Levy (CIL)

10.8.1 A legal test for the imposition of planning obligations was introduced by the Community Infrastructure Levy Regulations 2010. These provide that a planning obligation may only constitute a reason for granting planning permission for the development if the obligation is:

- (a) necessary to make the development acceptable in planning terms,
- (b) directly related to the development; and
- (c) fairly and reasonably related in scale and kind to the development.

10.8.2 The proposal is for Private Rented Sector (PRS) flats. The National Planning Policy Guidance set out above makes a distinction for PRS provision which states that: 'The economics of such schemes differ from build to sale and should be determined on a case by case basis. To help ensure these schemes remain viable while improving the diversity of housing to meet local needs, local planning authorities

should consider the appropriate level of planning obligations, including for affordable housing, and when these payments are required’.

10.8.3 It is accepted that PRS housing has specific characteristics, brought about through its funding model, which may make it unsuitable for the delivery of on-site Affordable Housing provision through a Housing Association or Registered Provider (RP). The applicant has proposed to pursue three options in turn starting with on-site provision with a Housing Association through to an off-site contribution if on-site provision is not possible. In some PRS schemes it has been possible to secure on site provision of affordable flats to be managed directly by the PRS provider at the Council’s benchmark rents, in perpetuity, with compliance to a Local Lettings Policy. If this option is not possible, there may be a commuted sum payable, with no flats to be sold on for at least 10 years from completion. The flats would be provided in a pro-rata mix of one, two and three bed flats. This will be controlled through the S106 agreement. This is considered to be an acceptable approach given the March 2017 Executive Board report.

10.8.4 Adopted policies are likely to result in the following necessary Section 106 matters:

- Affordable Housing 5% in a pro-rata mix of flats in a tiered approach starting with
 - (i) on-site provision managed by a Housing Association,
 - (ii) on-site provision managed by the PRS provider in accordance with the Council’s benchmark rents and a Local Lettings Policy in perpetuity, and
 - (iii) a commuted sum based on the Council’s benchmark rents as set out in the March 2017 Executive Board report
- Sustainable Travel Fund £14,803
- Car club trial provision £10,000
- Amendments to traffic regulation order to replace 3 pay and display bays to accommodate loading and car club £7,500
- Revenue compensation for loss of 3 pay and display bays £18,000
- Travel plan monitoring fee £2505
- Public access to routes at ground level
- Cooperation with local jobs and skills initiatives

10.8.5 The proposal would be subject to the Community Infrastructure Levy (CIL) and this has been calculated as £30,731.47

11.0 Conclusion

11.1 This scheme is a significant regeneration opportunity that will contribute to the delivery of the Council’s adopted place-making vision for the transformation of the Holbeck area of Leeds South Bank, and high quality development which will bring with it a number of new sustainable dwellings. The scheme would provide a range of house types and sizes, and provide construction jobs. The proposed redevelopment would also enhance the character of the surrounding area, by regenerating a prominently-located unsightly and under-utilised brownfield site that has been vacant for many years. Section 66(1) of the Planning (Listed Buildings and Conservation Areas) Act 1990 states that Local Planning Authorities (LPAs) must have special regard to the desirability of preserving the building or its setting or any features of special architectural or historic interest which it possesses and pay special attention to the desirability of preserving or enhancing the character and appearance of a Conservation Area, and it considered that the proposal would enhance the setting and significance of the Grade I listed building Temple Mill and the character and appearance of the nearby Holbeck Conservation Area. The proposal is on balance in accordance with the Development Plan and national planning policy as described above.

Background Papers

Application file 17/02666/FU

Appendix 1

Draft Conditions for 17/02666/FU

Appendix 2

Proposed Site Layout Plan

Appendix 1

Draft Conditions for 17/02666/FU

- 1) The development hereby permitted shall be begun before the expiration of three years from the date of this permission.

Imposed pursuant to the provisions of Section 91 of the Town and Country Planning Act 1990 as amended by Section 51 of the Planning and Compulsory Purchase Act 2004.

- 2) The development hereby permitted shall be carried out in accordance with the plans listed in the Plans Schedule

For the avoidance of doubt and in the interests of proper planning.

- 3) The approved Phase I Desk Study report indicates that a Phase II Site Investigation is necessary, and therefore development shall not commence until a Phase II Site Investigation Report has been submitted to, and approved in writing by, the Local Planning Authority.

Where remediation measures are shown to be necessary in the Phase II Report and/or where soil or soil forming material is being imported to site, development shall not commence until a Remediation Statement demonstrating how the site will be made suitable for the intended use has been submitted to, and approved in writing by, the Local Planning Authority. The Remediation Statement shall include a programme for all works and for the provision of Verification Reports.

To ensure that the presence of contamination is identified, risks assessed and proposed remediation works are agreed in order to make the site 'suitable for use'.

- 4) If remediation is unable to proceed in accordance with the approved Remediation Statement, or where significant unexpected contamination is encountered, the Local Planning Authority shall be notified in writing immediately and operations on the affected part of the site shall cease. An amended or new Remediation Statement shall be submitted to, and approved in writing by, the Local Planning Authority prior to any further remediation works which shall thereafter be carried out in accordance with the revised approved Statement.

To ensure that any necessary remediation works are identified to make the site suitable for use.

- 5) Remediation works shall be carried out in accordance with the approved Remediation Statement. On completion of those works, the Verification Report(s) shall be submitted to the Local Planning Authority in accordance with the approved programme. The site or phase of a site shall not be brought into use until such time as all verification information has been approved in writing by the Local Planning Authority.

To ensure that the remediation works are fully implemented as agreed and the site has been demonstrated to be suitable for use.

- 6) Prior to the commencement of development, a Statement of Construction Practice shall be submitted to and approved in writing by the Local Planning Authority. The Statement of Construction Practice shall include full details of:

- a. the methods to be employed to prevent mud, grit and dirt being carried onto the public highway from the development hereby approved;
- b. measures to control the emissions of dust and dirt during construction;
- c. location of site compound and plant equipment/storage;
- d. location of contractor and sub-contractor parking;
- e. how this Statement of Construction Practice will be communicated by the developer to local residents
- f. Construction works and delivery hours shall be restricted to 0800-1800 hours Monday to Friday, 0800-1300 hours on Saturdays, with no works on Sundays and Bank Holidays.

The approved details shall be implemented at the commencement of work on site, and shall thereafter be retained and employed until completion of works on site. The Statement of Construction Practice shall be made publicly available for the lifetime of the construction phase of the development in accordance with the approved method of publicity.

In the interests of residential amenity of occupants of nearby property

- 7) The development permitted by this planning permission shall be carried out in accordance with the approved Flood Risk Assessment (FRA) reference no: 17028, Manor Road Leeds, Flood Risk Assessment & SuDS Statement, dated April 2017 and the following mitigation measures detailed within the FRA:
 - a. Ground floor finished levels shall be set no lower than 28.4m above Ordnance Datum (AOD).
 - b. There shall be no sleeping accommodation on the ground floor.
 - c. Safe routes into and out of the site to an appropriate safe haven shall be provided.
 - d. preparation of a flood risk warning and evacuation plan

To reduce the risk of flooding to the proposed development and future occupants, reduce the risks of flooding to occupants in a flood event, and to ensure safe access and egress from and to the site.

- 8) Building operations shall not commence until a drainage scheme (i.e. drainage drawings, summary calculations and investigations) detailing the surface water drainage works has been submitted to and approved in writing by the Local Planning Authority. The surface water discharges should be restricted to 5 l/s unless otherwise otherwise agreed in writing with the Local Planning Authority and surface water drainage scheme shall be accord with the Leeds City Councils Minimum Development Control Standards for Flood Risk. The works shall be implemented in accordance with the approved scheme before the development is brought into use.

To ensure sustainable drainage and flood prevention.

- 9) No piped discharge of surface water shall take place until works to provide a satisfactory outfall for surface water have been completed in accordance with details to be submitted to and approved by the local planning authority before development of that phase commences.

To ensure that the site is properly drained and surface water is not discharged to the foul sewerage system which will prevent overloading.

- 10) Prior to the commencement of building works, a sample panel of all external facing materials, roofing and glazing types to be used shall be constructed on-site and approved in writing by the Local Planning Authority. The external cladding and glazing

materials shall be constructed in strict accordance with the sample panel(s) which shall not be demolished prior to the completion of the development.

In the interests of visual amenity.

- 11) No building works shall be commenced until full 1 to 20 scale working drawing details of the following have been submitted to and approved in writing by the Local Planning Authority:
- a. soffit, roof line and eaves treatments
 - b. junctions between materials
 - c. each type of window bay proposed
 - d. ground floor frontages

The works shall be carried out in accordance with the details thereby approved.

In the interests of visual amenity and the character of the surrounding area.

- 12) No surfacing works shall take place until details and samples of all surfacing materials have been submitted to and approved in writing by the Local Planning Authority. Such materials shall be made available on site prior to the commencement of their use, for the inspection of the Local Planning Authority who shall be notified in writing of their availability. The surfacing works shall be constructed from the materials thereby approved and completed prior to the occupation of the building.

In the interests of visual amenity in accordance with Leeds Core Strategy Policy P12, Saved UDP Review Policies GP5 and LD1, and the NPPF.

- 13) Landscaping works shall not commence until full details of both hard and soft landscape works, including an implementation programme, have been submitted to and approved in writing by the Local Planning Authority. Hard landscape works shall include
- a. proposed finished levels and/or contours,
 - b. boundary details and means of enclosure,
 - c. other vehicle and pedestrian access and circulation areas,
 - d. hard surfacing areas,
 - e. minor artefacts and structures (e.g. furniture, play equipment, refuse or other storage units, signs, lighting etc.),
 - f. proposed and existing functional services above and below ground (e.g. drainage, power cables, communication cables, pipelines etc., indicating lines, manholes, supports etc.).

Soft landscape works shall include

- g. planting plans
- h. written specifications (including soil depths, soil quality, tree pits, cultivation and other operations associated with plant and grass establishment) and
- i. schedules of plants noting species, planting sizes and proposed numbers/densities.

All hard and soft landscaping works shall be carried out in accordance with the approved details, approved implementation programme and British Standard BS 4428:1989 Code of Practice for General Landscape Operations. The developer shall complete the approved landscaping works and confirm this in writing to the Local Planning Authority prior to the date agreed in the implementation programme.

To ensure the provision and establishment of acceptable landscape in accordance with Leeds Core Strategy Policy P12, Saved Leeds UDP Review (2006) policies GP5, N23, N25 and LD1.

- 14) A landscape management plan, including long term design objectives, management responsibilities and maintenance schedules shall be submitted to and approved in writing by the Local Planning Authority prior to the occupation of the development. The landscape management plan shall be carried out as approved.

To ensure successful aftercare of landscaping.

- 15) If within a period of five years from the date of the planting of any tree/hedge/shrub that tree/hedge/shrub, or any replacement, is removed, uprooted or destroyed or dies, or becomes, in the opinion of the Local Planning Authority, seriously damaged or defective, another tree/hedge/shrub of the same species and size as that originally planted shall be planted in the same location as soon as reasonably possible and no later than the first available planting season, unless otherwise agreed in writing by the Local Planning Authority.

To ensure maintenance of a healthy landscape scheme

- 16) No building works shall take place until details for the provision of off-site highways works shown on the submitted plan reference 17027-GA-01-C have been submitted to and approved in writing by the Local Planning Authority for inclusion in the section 278 Highways Agreement or to be secured by such other procedure as may be agreed between the applicants and the Local Planning Authority. Works shall be completed prior to the occupation of the development, unless otherwise agreed in writing by the Local Planning Authority.

In the interests of pedestrian and vehicular safety.

- 17) Notwithstanding the details shown on the plans hereby approved and prior to the commencement of building works, full details of the facilities for the parking of cycles for residents shall be submitted to and approved in writing by the Local Planning Authority. The development shall not be brought into use until the cycle parking facilities thereby approved have been provided. The facilities shall thereafter be retained and maintained as such.

In in the interest of promoting sustainable travel.

- 18) Prior to the installation of any extract ventilation system or air conditioning plant, details of such systems shall be submitted to and approved in writing by the Local Planning Authority. Any external extract ventilation system/air conditioning plant shall be installed and maintained in accordance with the approved details. The systems shall limit noise to a level at least 5dBA below the existing background noise level (L90) when measured at the nearest noise sensitive premises including flats within the development with the measurements and assessment made in accordance with BS4142:1997.

In the interests of visual and residential amenity.

- 19) No building works shall commence until a detailed acoustic assessment of the proposed building and a noise insulation scheme, including details of any necessary mechanical ventilation equipment, have been submitted for formal written approval by the Local Planning Authority. The scheme shall be designed to protect the amenity of

the residents of the proposed dwellings from noise from surrounding roads, adjoining uses and any plant or equipment associated with the approved building. The noise insulation scheme for the development shall be designed to achieve internal noise levels in living spaces not exceeding 35dB(A) and 30dB(A) in bedrooms at night, with peak levels kept below 45dB(A). The approved scheme shall be implemented prior to occupation of the dwellings and retained thereafter

In the interests of residential amenity

- 20) Prior to the commencement of building operations an updated Sustainability Statement shall be submitted to and approved in writing by the Local Planning Authority for that phase, which shall include a detailed scheme comprising:

- a. a recycled material content plan (using the Waste and Resources Programme's (WRAP) recycled content toolkit)
- b. a Site Waste Management Plan (SWMP)
- c. an energy plan showing the amount of on-site energy produced by the selected Low and Zero Carbon (LZC) technologies and that it produces a minimum of 10% of total demand, unless otherwise agreed in writing by the Local Planning Authority
- d. details that demonstrate a minimum of 20% carbon dioxide emissions reduction against Part L of the 2013 Building Regulations and meet the Low Water Usage Target

The development shall be carried out in accordance with the details as approved above;

- e. Within 6 months of the occupation of the development a post- construction review statement shall be submitted by the applicant including formal accreditation and approved in writing by the Local Planning Authority

The development and buildings comprised therein shall be maintained and any repairs shall be carried out all in accordance with the approved detailed scheme and post-completion review statement or statements.

To ensure the adoption of appropriate sustainable design principles

- 21) Prior to first occupation a scheme detailing the method of storage and disposal of litter and waste materials, including recycling facilities, shall be submitted to and approved in writing by the Local Planning Authority. The details shall include a description of the facilities to be provided including, where appropriate, lockable containers and details of how the recyclable materials will be collected from the site with timescales for collection. The approved scheme shall be implemented before the development hereby permitted is brought into use and no waste or litter shall be stored or disposed of other than in accordance with the approved scheme.

In the interests of amenity and to promote recycling.

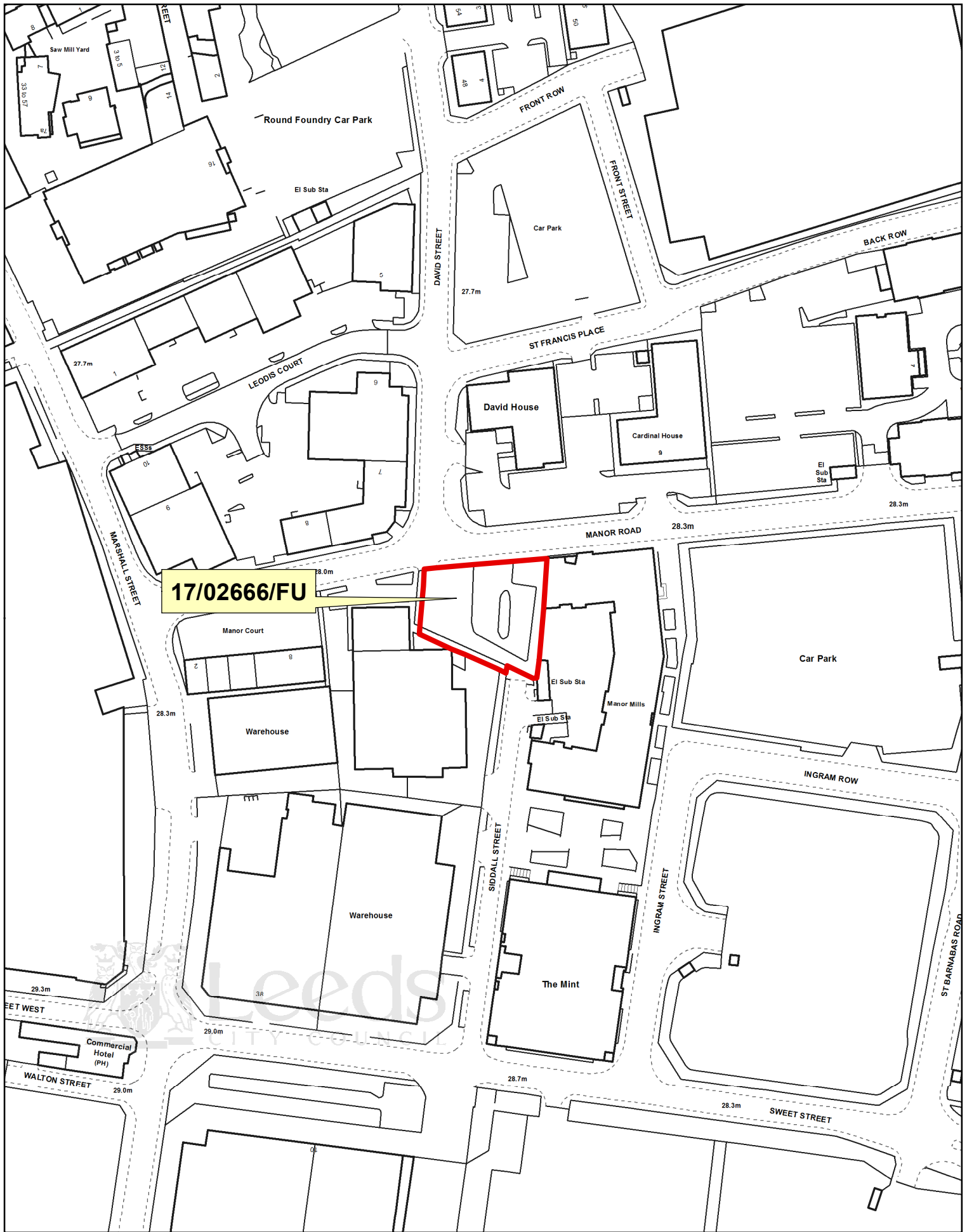
- 22) Notwithstanding details shown on the plans hereby approved the gradient of pedestrian ramps shall meet BS8300:2009+A1:2010. Works shall be carried out in accordance with the approved details and retained as such thereafter.

In the interests of accessibility for all.

- 23) No works to or removal of trees or shrubs shall take place between 1st March and 31st August inclusive, unless a competent ecologist has undertaken a careful, detailed check of vegetation for active birds nests immediately before (within 24

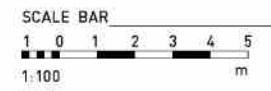
hours) the works commence and provided written confirmation that no birds will be harmed and/or that there are appropriate measures in place to protect nesting bird interest on site. Any such written confirmation should be submitted to the Local Planning Authority within 3 days of such works commencing.

To protect nesting birds in vegetation and built structures in accordance with the Wildlife and Countryside Act 1981 (as amended) and BS 42020:2013.



CITY PLANS PANEL



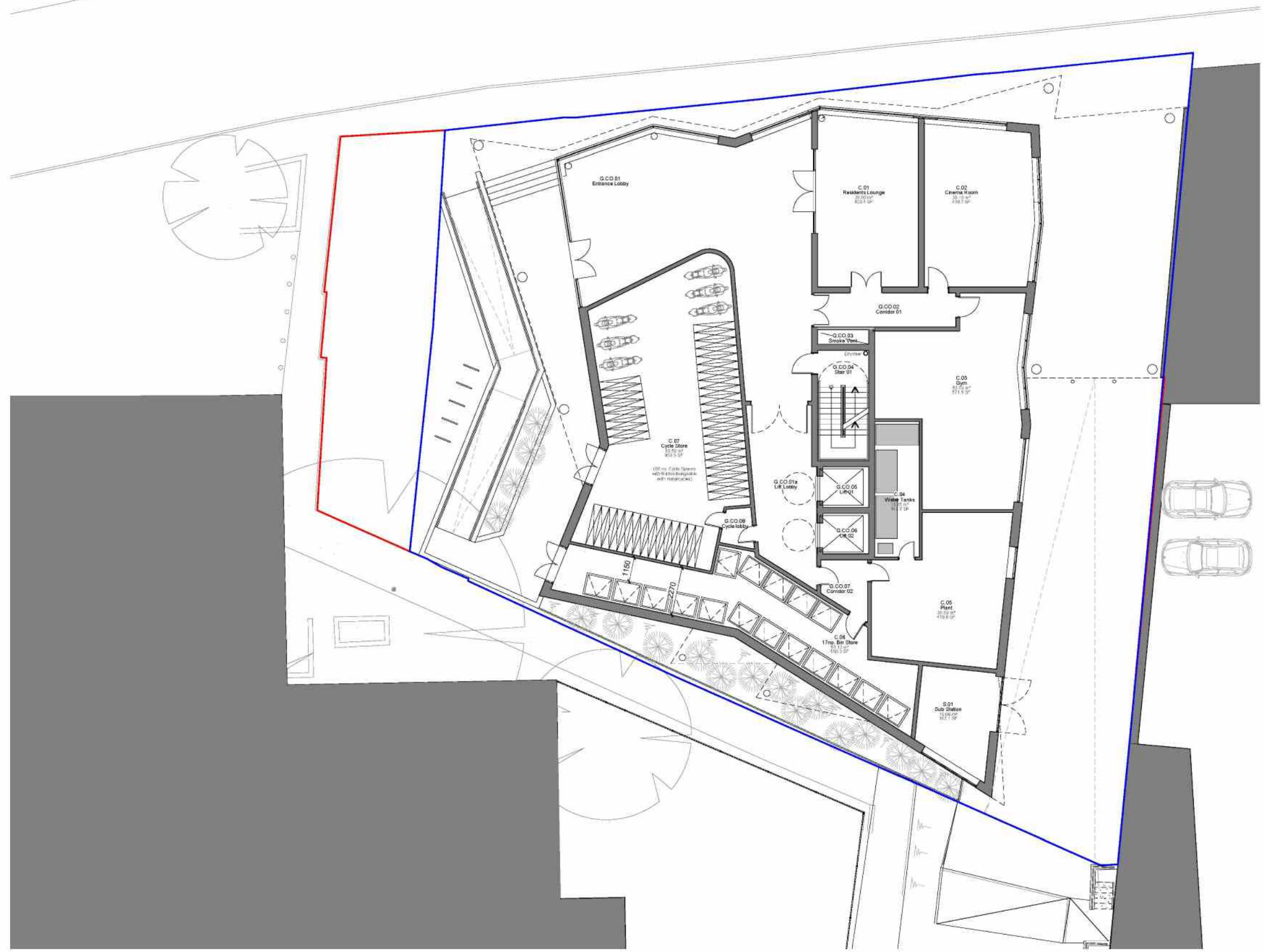


NOTE :
Do not scale from this drawing, use figured dimensions only. Scale bar shown for reference only.

NORTH POINT



Application boundary
Ownership boundary



Rev	Description	Date	Dr	App
G	Red line amended	20.09.17		
F	Entrance and cycle store amended for wind mitigation	18.09.17		
E	Ownership line amended	26.04.17		
D	Cycle store and ramp amended as highway consultant comments	24.04.17		
C	Ramp wall amended	03.4.017		
B	Site context added	24.02.17	PC	
A	Room no.s added	17.02.17		

original by	date created	Approved by
Author	02/14/17	Approver

NICK BROWN : ARCHITECTS
 88 Cuts Landing
 26-28 The Cuts
 Leeds
 LS2 7EW
 t: 0115 323 5733
 www.nickbrownarchitects.com

project
Proposed Residential Development for Citylife, Manor Road, Leeds

drawing title
Proposed Ground Floor GA Plan

project number
2016-107

drawing number	revision
P200	G

scale	title status
1 : 100 @A1	Prelim

NOTES
 Contractor must verify all dimensions on site before commencing any work on this drawing.
 Check all dimensions before commencing work on site. If this drawing contains the quantities table, the quantities are for information only and are not to be used for procurement.
 Check figures and dimensions to be used on this drawing. Do not scale off this drawing.
 Drawing based on Citylife Leeds and Leeds City Council drawings. Design and construction subject to all planning, structural, services, fire, mechanical, plumbing and building regulations and approvals.
 Check with the Council for Design & Management Agreement (D&M) to be used to provide a Design Plan for the final production.
 The drawings Copyright reserved and are approved without consent of Nick Brown Architects Ltd.